

NOTICE

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

8. Description of Work Accomplished

(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

N3866M

July 23, 2023

Nationality and Registration Mark

Date

Installed Brackett airfilter assembly BA-4106 in accordance with Brackett Instruction Sheet p.n. 4105 and STC SA71GL.
END.

Additional Sheets Are Attached

NOTICE

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

8. Description of Work Accomplished

(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

N3866M

July 23, 2023

Nationality and Registration Mark

Date

Installed ICOM IC-A220 transceiver in accordance with ICOM IC-A220 Installation Manual and AC 43.13 - 1B, Chapter 10, 11, & 12 and AC 43.13 - 2A, Chapter 1,2, & 3. Updated equipment list and weight and balance.

END.

Additional Sheets Are Attached

NOTICE

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

8. Description of Work Accomplished

(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

N3866M

December 13, 2012

Nationality and Registration Mark

Date

Removed TLC 100 Loran and Narco 11A Comm.

Installed IIMorrow Apollo GX65 GPS, p.n. 430-6050-801 and A-33 Antenna.

The instrument panel has been placarded "GPS for VFR use only" in clear view of pilot.

All work done in accordance with the following documents.

Apollo Model GX65 GPS Installation Manual 560-0960-0 Rev. 01, dated 11/17/1997.

Apollo GX65 GPS Configuration Index 560-1001-03, dated 5/14/1999.

AC 43.13-1B, Chapter 11

AC 43.13-2A, Chapters 1,2&3.

FAR 23.1301, 23.1309, and 23.143

AC 20-138, Paragraph 7(c)(2)(i)(ii) and (iii)

Updated equipment list and weight and balance. See next page for Instructions for Continued Airworthiness.

End of Page 2.

Additional Sheets Are Attached

NOTICE

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

8. Description of Work Accomplished

(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

Installed North River Aviation brake booster in accordance with North Aviation Drawing No. 8400 and STC SA3633NM. Weight change negligible. **END.**

Additional Sheets Are Attached

NOTICE

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

8. DESCRIPTION OF WORK ACCOMPLISHED (If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

1. Installed Terra Corporation Model T.L.C. 100 Loran C in accordance with Manufacturers Owner/Installation Manual, and AC43.13 1a-2a.
2. Installed Narco Com 11A in accordance with Manufacturers Instructions and AC43.13 1a and 2a.
3. See updated weight and balance data dated 05-05-89.

/-----END-----/

ADDITIONAL SHEETS ARE ATTACHED

NOTICE

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

8. Description of Work Accomplished

(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

1. Aircraft modified by installation of S.T.C. #SA1008WE.
2. Removed original cloth covering.
3. Installed polu-fiber covering materials in accordance with Poly-fiber Covering Procedure Manual No. 1, Revision Thirteen dated January, 1983.

Additional Sheets Are Attached

NOTICE

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

8. Description of Work Accomplished

(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

The following ribs were replaced in the right wing in accordance with Univair Aircraft Corp. S.T.C. SA1535NM.

1. Right tip rib assy. Univair PN 007-U1203-1.
2. Outboard aileron bay rib right hand #15 Univair PN 007-U1236-05.
3. 3/4 rib right hand #14 Univair PN 007-U1237-06.
4. 3/4 rib #13 Univair PN 007-U1237-01.
5. 3/4 rib #11 Univair PN 007-U1237-02.
6. Butt rib right hand #1 Univair PN 007-U1235-01.

Ribs were installed using new Parker Kalon No. 4 x 1/4 type A stove head screws.

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

NOTICE

8. Description of Work Accomplished

(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

Installed PA-18 wing flaps in accordance with S.T.C. No. SA578AL using parts list and installation instructions dated March 26, 1976 except for the following modifications.

- A. Steps 15, 16 and 17 were omitted and flaps were extended 3 inches inboard.
 - 1. Leading edge inboard of inboard flap hanger was removed.
 - 2. First rib and side brace removed.
 - 3. Flap spar was extended 3 inches in accordance with AC 43.13-1A Chapter 2, Section 3 using .025 2024-T3 sheet stock for reinforcing plate.
 - 4. New leading edge constructed from .020 3003-H14 to same as original dimensions except 3 inches longer.
 - 5. Reinstalled rib and side brace removed in step 2.
 - 6. Extended trailing edge 3 inches using trailing edge removed from wing during modification for flap installation. 4 each Parker Kalon No. 4 x 1/4 type "A" stove head screws used to attach extension.
 - 7. All riveting done in accordance with AC 43.13-1A Chapter 2, section 3 with AN-470A-3 rivets, "see attached photos #1 and #2."
- B. Two fairleads installed on #2 rib assembly of both wings to insure that flap return spring, control link and bell crank do not interfere with the aileron control cables when each system is operated.
 - 1. For each wing two PN-40071-00 split fairleads and two PN40081-00 fairlead snap rings were used.
 - 2. Wing rib fairlead brackets and "U" brackets are constructed from .025 2024-T3 Alclad sheet stock. "See attached drawing for location of fairleads."

Additional Sheets Are Attached

1 of 4

NOTICE

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

Description of Work Accomplished

(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

Modified aircraft by installing PA-18 trim system.

1. Removed stabilizer adjustment screw pulley, stabilizer adjustment crank pulley and stabilizer adjustment cable. All other PA-12 trim system parts unchanged.
2. Installed the following PA-18 trim system parts in place of PA-12 parts removed.
 - A. Stabilizer adjustment screw pulley P.N. 12982-00.
 - B. Stabilizer adjustment crank pulley P.N. 12983-00.
 - C. Stabilizer adjustment cable P.N. 41671-19.
3. Installed cable guard for trim screw pulley and trim crank pulley made from 1/8 inch 4130 steel welding rods. See photos #1 and #2.
4. Welded brackets to airframe to support Idler pulleys made from .050 4130 steel. See "A" photo #1.
5. Pulleys P.N. 43001-00 attached to brackets using for each, two washers P.N. 80122-18, one bushing P.N. 80012-04, one bolt P.N. AN3-6, one nut P.N. AN310-3, one AN960-3 washer and a cotter key AN380-2.
6. Spring loaded pulley P.N. 80421-00 attached to airframe using one bracket P.N. 11692-05, two washers P.N. 80122-18, one bushing P.N. 80012-04, one bolt AN3-6, one nut AN310-3, one washer AN960-3, one cotterkey AN380-2, one flathead pin AN396-31, one spring 81411-02 and a bracket welded to upper right longeron made from .090 4130 steel. See "B" photo #1.
7. Idler pulley P.N. 80421-00 installed below trim crank pulley using two washers P.N. 80122-18, one bushing P.N. 80012-04, one bolt P.N. AN3-6, one nut P.N. AN310-3 cotterkey P.N. AN380-2 and a bracket welded to structure made from .050 4130 steel. See photo #2.
8. Two AN380-2 cotter keys used for cable guards on trim crank idler pulley. See photo #2.
9. All welding done in accordance with AC43.13-1A Chapter 2, section 2.

NOTICE

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

8. Description of Work Accomplished

(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

Modified aircraft by installing skylight in accordance with drawings dated 04-10-74 from Croswinds, Inc.

1. Fabricated front and rear angles from .032 thick 4130 steel. See drawing sheet numbers 3 of 5 and 4 of 5.
2. Welded angles in place. See drawing sheet numbers 3 of 5 and 4 of 5. Welding done in accordance with AC43.13 Chapter 2, Section 2.
3. Cut skylight from clear 1/8 inch thick "Lexan" sheet.
4. Attached skylight to front and rear angles using AN 526-1032R12 screws and MS 21078-3 nut plates.
5. Attached skylight to existing superstructure using AN526-832 screws, AN 363-832 nuts and AN 970-3 washers.
6. See drawing sheets numbers 1 of 5 through 5 of 5 for other details.

NOTICE

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

8. Description of Work Accomplished

(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

Modified aircraft interior by removing doped fabric interior and fabricating aluminum sheet interior.

1. Removed old doped fabric interior.
2. Fabricated interior panels from Alclad 2024-T3 .020 thick.
3. Panels attached to airframe using Parker Kalon 4x $\frac{1}{4}$ type "A" screws around entrance door and above windows. Type A AN530 6R6 round head screws used to attach panels to wood floor.
4. Automotive type trunk felt was glued to panels using 3M brand P.N. 08080 general trim adhesive.
5. See attached photos #1, #2 and #3.

NOTICE

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

8. Description of Work Accomplished

(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

Aircraft modified by removing original baggage compartment and supports including headliner and installing compartment of our own design.

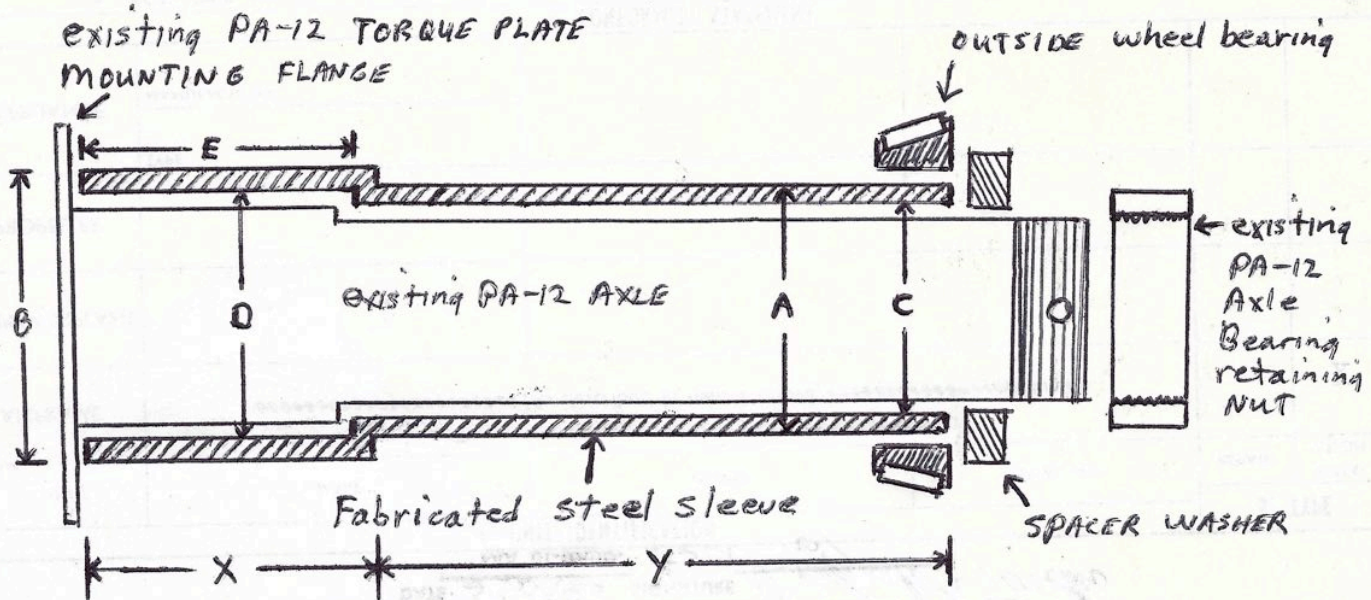
1. Cut old baggage compartment support out. See Figure 1.
2. Welded five tabs constructed of 4130 steel, x .090 nine inches up from bottom longeron. See Figure 2, Figure 1 and photo #3 and #4 for locations.
3. Installed nut plates PN AN366-1032 in each tab using counter sunk screws and self-locking steel nuts. See Figure 2.
4. Removed complete battery installation including door box, starter solinoid and main fuse block.
5. Relocated complete battery installation except for access door. Battery access will be through back of new baggage compartment. See Figure 1 for new location and detail drawing "A" for installation; also photo #2.
6. The first former aft the rear wing butt fitting was modified to allow for larger baggage area. See figure 3.
7. Five tabs were welded to horizontal tube located 141.5 inches aft of the datum and 21.5 inches up from lower longeron. See photo #2.
8. Compartment encloser constructed from light weight wood paneling and assembled using "U" type tinnerman nuts and round head sheet metal screws. Enclosure corners constructed from 2024-T3 .032 alclad sheet stock. See Figure 4 and photo #'s 1, 2, 3 and 4.
9. Rerouted fuel line from right wing tank around baggage compartment. See photos 2, 3 and 4.
10. Deflected trim cables around corner of baggage compartment using two Piper rubbing blocks Part No. 40721-00. See photo #4. Brackets for rubbing blocks are welded to aircraft structure. See Figure #5.
11. All welding done in accordance with AC 43.13-1A, Chapter 2, section 2.
12. Baggage area fitted to side windows by constructing window sills from soft aluminum .020 thick. See photo #1.
13. Aileron cables prevented from chafing on aluminum window sills by installing four split fairlead, part number 40071-00 and 40081-00 fairlead snap rings. See photo #1.
14. Baggage compartment placarded same as original, "capacity 41 lbs." Until aircraft is completed and can be weighed for accurate weight and balance revision.

NOTICE

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

8. DESCRIPTION OF WORK ACCOMPLISHED (If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

Aircraft modified by removing existing wheels, tires and brake assembly and installing fabricated steel axle sleeves on both existing main gear axles with PA 28-160 wheel assembly Piper Part # 451-769, brake assembly Part # 451-718, torque plate assembly Part # 755-818 and 8.00-6 tires and tubes. The fabricated axle sleeves conform exactly to the dimensions of PA 28-160 stub axle Part # 63303-02(03) to accommodate wheel assembly Part # 451-769.



Dimensions ABXY conform exactly to Piper Part # 63303-02 (03) stub axle.

Dimensions CD allows for shrink fit over PA-12 axle.

Dimension E allows fabricated steel axle sleeve to butt on existing PA-12 torque plate mounting flange.

Spacer washer provides retention for outside wheel bearing and to take up clearance between outside of wheel bearing and existing PA-12 axle bearing retaining nut.

ADDITIONAL SHEETS ARE ATTACHED

NOTICE

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

8. DESCRIPTION OF WORK ACCOMPLISHED (If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

Aircraft modified in accordance with STC SA 83 AL except installed Lycoming O320 B2B engine. This engine has the same size and configuration and is the same weight as is the Lycoming O320 C3A engine as approved with the STC SA 83 AL. The engine tach shall be placarded at 2600 R.P.M. maximum which will not exceed 150 H.P. Reference Lycoming O320 Operator's Manual dated March, 1973, figure 3-5, page 3-17. The aircraft shall be placarded for 100/100LL fuel.

ADDITIONAL SHEETS ARE ATTACHED

NOTICE

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

8. DESCRIPTION OF WORK ACCOMPLISHED (If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

1-10-65
Removed unbalanced tail surfaces and installed balanced tail surfaces. Install elevators, Piper P/N 12789-00 and 12789-01 and stabilizers, Piper part no. 12790-00 and 12790-01 per Wilbur Repair Service instructions sheet No. 1 dated 5-18-61 and STC SA5-33.

ADDITIONAL SHEETS ARE ATTACHED